

SLOVENSKI STANDARD

oSIST prEN 13044-1:2009

01-februar-2009

Swap bodies - Marking - Part 1: Markings for identification

Swap bodies - Marking - Part 1: Markings for identification

Wechselbehälter - Markierung - Teil 1: Markierungen für die Identifizierung

Caisses mobiles - Marquage - Partie 1: Identification de propriétaire

Ta slovenski standard je istoveten z: prEN 13044-1

<https://standards.iteh.ai/catalog/standards/sist/492d4fce-34dd-49c7-a534-6814320d4c46/sist-en-13044-1-2011>

ICS:

35.240.60	Uporabniške rešitve IT v transportu in trgovini	IT applications in transport and trade
55.180.10	X^ } æ ^} • \ á [} c b ^ i k̃	General purpose containers

oSIST prEN 13044-1:2009

en

EUROPEAN STANDARD
NORME EUROPÉENNE
EUROPÄISCHE NORM

DRAFT
prEN 13044-1

November 2008

ICS 55.180.10; 35.240.60

Will supersede EN 13044:2000

English Version

Swap bodies - Marking - Part 1: Markings for identification

Caisses mobiles - Marquage - Partie 1: Marquages
d'identification

Wechselbehälter - Markierung - Teil 1: Markierungen für die
Identifizierung

This draft European Standard is submitted to CEN members for enquiry. It has been drawn up by the Technical Committee CEN/TC 119.

If this draft becomes a European Standard, CEN members are bound to comply with the CEN/CENELEC Internal Regulations which stipulate the conditions for giving this European Standard the status of a national standard without any alteration.

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Recipients of this draft are invited to submit, with their comments, notification of any relevant patent rights of which they are aware and to provide supporting documentation.

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COMITÉ EUROPÉEN DE NORMALISATION
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Foreword

This document (prEN 13044-1:2008) has been prepared by Technical Committee CEN/TC 119 “Swap bodies for combined goods transport”, the secretariat of which is held by DIN.

This document is currently submitted to the CEN Enquiry.

This document supersedes EN 13044:2000.

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Introduction

This standard contains the recommended method of marking *Intermodal Loading Units (ILU)* to meet current and future requirements.

The standard follows the format used in EN ISO 6346, the world wide accepted standard for marking and coding of marine freight containers. As the above standard can be applied, without alteration, to an ILU, the standard is not directly interchangeable with the ISO standard. However since the ILU are handled and transported in Europe in the same environment as ISO freight containers, the two standards are compatible.

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1 Scope

This European Standard provides a system for the identification and presentation of information about the ILU. The identification system is intended for general application, for example in documentation, control and communications (including automatic data processing systems), as well as for display on an ILU and other non ISO containers (i.e.: which dimensions and testing parameters differ from those defined by the applicable ISO standards) used in European transport.

The methods of displaying identification and certain other data (including operational data) on swap bodies by means of permanent marks are included.

This European Standard specifies:

- a) an ILU identification system with an associated system for verifying the accuracy of its use, having mandatory marks for the presentation of the identification system for visual interpretation, and
- b) a coding system for data on ILU size and type, with corresponding marks for their display;
- c) mandatory operational marks;
- d) physical presentation of the marks on the ILU.

This Part of EN 13044 specifies a system to identify the owner of the ILU, which includes an associated system for verifying the accuracy of its use.

This European Standard does not cover temporary operational marks of any kind, permanent marks, data plates, etc. which may be required by intergovernmental agreements, national legislation or non-governmental organisations other than CEN.

NOTE Some of the major international conventions whose container-marking requirements are not covered in this European Standard are as follows:

- International Convention for Safe Containers (UN/IMO 1992);
- Customs Convention on Containers 1956 and 1972;
- Customs Convention on International Movement of Goods under Cover of TIR Carnets (TIR-Convention) 1959 and 1975.

It should not to be assumed that this list is exhaustive.

This European Standard does not cover the display of technical data on Swap tanks (see EN 1432) nor does it, in any way, include identification marks or safety signs for items of cargo which may be carried in ILU.

2 Normative references

The following referenced documents are indispensable for the application of this document. For dated references, only the edition cited applies. For undated references, the latest edition of the referenced document (including any amendments) applies.

EN ISO 6346, *Freight containers – coding, identification and marking*

UIC 596-6, *Conveyance of road vehicles on wagons – Technical organisation - Conditions for coding combined-transport load units and combined-transport lines*

3 Terms and definitions

For the purposes of this document, the following terms and definitions apply.

3.1

Intermodal Loading Unit

ILU

loading unit suitable for European intermodal transport on road, rail, inland waterway and sea, which is not an ISO-container according to ISO 830

EXAMPLE swap body, semi trailer

3.2

Owner code

four letters code indicating the owner or the principal operator

4 Identification system and its associated marks

4.1 Identification system

The identification system for swap bodies shall consist only of the following elements, all of which shall be included:

- owner code : four letters
- serial number : six numerals
- check digit : one numeral

4.2 Identification of the owner

4.2.1 The identification of the owner of the ILU shall be achieved by using the owner code. The identification of the owner shall be unique and therefore it is necessary for all codes to be registered with the *International Union of Combined road rail transport companies* (UIRR). The assigned owner codes will be maintained and checked by UIRR.¹

4.2.2 The owner code for ILU shall consist of four capital letters where the last letter also identifies the unit as an ILU and shall consist of one of the five capital letters **A, B, D, E** or **K**.

4.2.3 The usage of this owner code is limited to the ILU that are circulating in the European region only.

4.2.4 The following letters as fourth letter of the owner code are reserved for international worldwide use according to ISO 6346 and assigned by BIC²:

- **U** for all freight containers;
- **J** for detachable freight container-related equipment;
- **Z** for trailers and chassis.

¹ UIRR scrl - 31 rue Montoyer bte 11 B-1000 Bruxelles; Fax +32 2 521 63 93; E-Mail: headoffice.brussels@uirr.com

² BIC: Bureau International des Containeurs

4.3 Serial number

The serial number for a swap body shall consist of six Arabic numerals. If the series of significant numerals does not total six, they shall be preceded by sufficient zeros to make up six numerals.

EXAMPLE series of significant numerals is 1234, serial number is 001234

4.4 Check digit

The check digit provides a means of validating the transmission accuracy of the owner code and serial number and shall be determined as in Annex A. The check digit shall validate the owner code and serial number of the ILU.

5 Physical display of marks

5.1 Size and colour of marks

The letters and numerals of the owner code, serial number and check digit shall not be less than 100 mm high. All characters shall be of proportionate height and width. They shall be durable and in a colour contrasting with that of the ILU.

5.2 Layout and location of marks

5.2.1 General

The requirements of this clause are applicable particularly to the 'closed box' type of ILU. For other ILU types, all possible practicable steps should be taken to follow the marking layout and location given for the 'closed box' type.

NOTE When it is not practical to follow the requirements of this clause EN ISO 6346 should be considered by preference to any other.

5.2.2 Layout of marks

The layout of the owner code serial number and check digit on the ILU shall preferably be one single horizontal line. Where constructional features of the ILU dictate otherwise, the layout may differ from the horizontal.

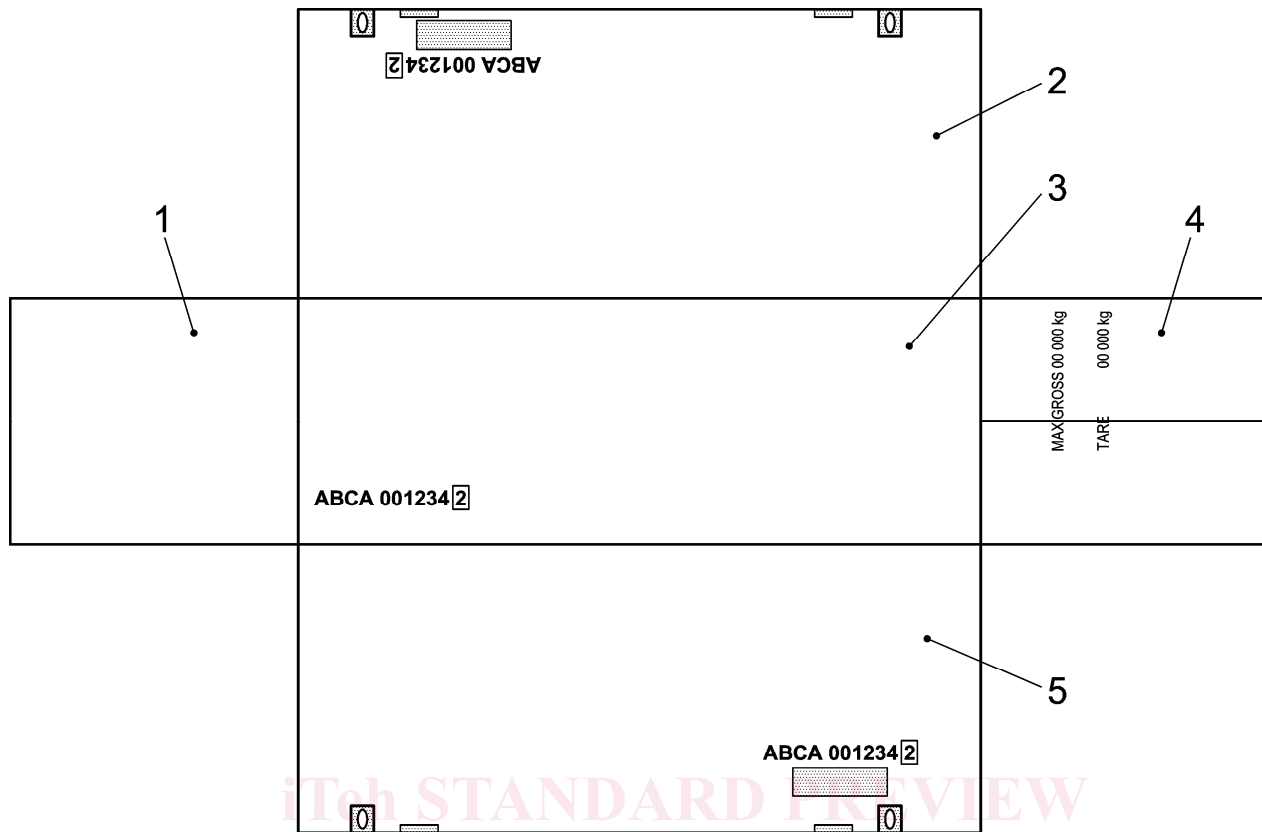
The owner code shall be joined together and shall be separated from the serial number by at least one character space. The serial number shall be separated from the check digit also by one character space and the check digit shall be displayed in a box.

An ILU, having a unique registered identification of its owner "ABCA" and a serial number "001234" and the check digit "2" shall be marked as follows:

ABCA 001234 2

5.2.3 Location of marks

The mandatory identification marks of Clause 3, which are the owner code, check digit, serial number, shall be positioned on the ILU as far as practicable as shown in Figure 1. It shall be present at least on both side walls and on the roof. Optionally the marks may be presented at the end wall.



- Key:
- 1 end wall
 - 2 right side wall
 - 3 roof
 - 4 door end (rear end wall)
 - 5 left side wall

Figure 1 — Location of markings